



January 29-30, 2025

# I-94 East-West Freeway Project

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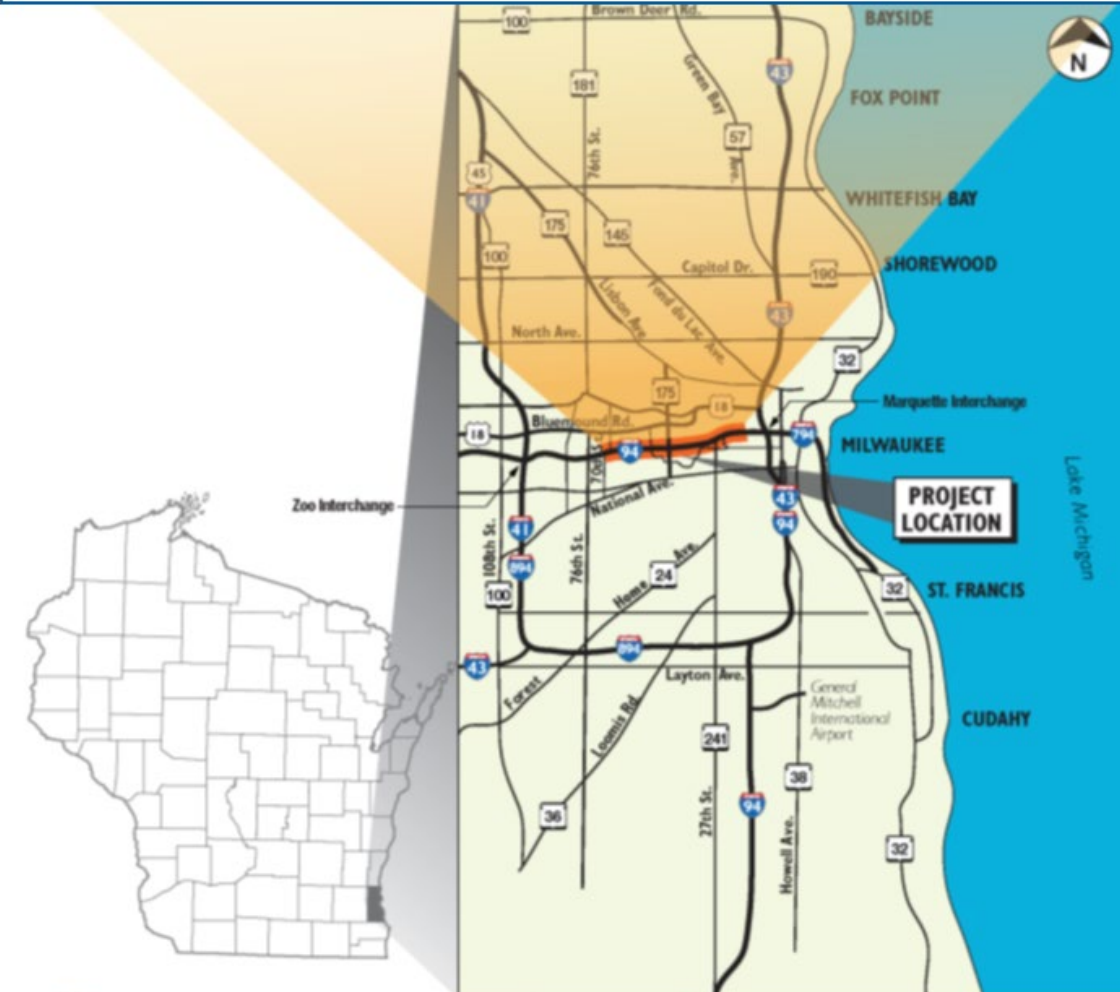
Wisconsin Department of Transportation



# PROJECT CORRIDOR AND BACKGROUND

## PROJECT LOCATION

- Interstate 94 from 70th Street to 16th Street; WIS 175 from Blue Mound Road to just south of American Family Field
- Located entirely in the City of Milwaukee; mitigation projects also in West Allis and West Milwaukee
- Fully built urban corridor
- Connects the major investments of the Marquette Interchange and Zoo Interchange



# COMPARING MEGA-PROJECTS

	I-94 East-West	Zoo Interchange	Marquette Interchange
Freeway miles	3.5 miles	9 miles	4 miles
Square foot of bridges	677,300 (42 bridges)	1.3 million	1.9 million
Length of retaining walls	8.9 miles (67 retaining walls)	11 miles	4.9 miles
Open to traffic	Early 1960s	1964	1969
Age at reconstruction	~60	50	35

## BACKGROUND

- **2012-16** – I-94 East-West Freeway Corridor study, recommended alternative, FHWA issues Record of Decision, approval rescinded in 2017 when project de-funded
- **July 2020** – project restarted
- **March 2024** – Federal approval - signed Supplemental Final EIS and Record of Decision



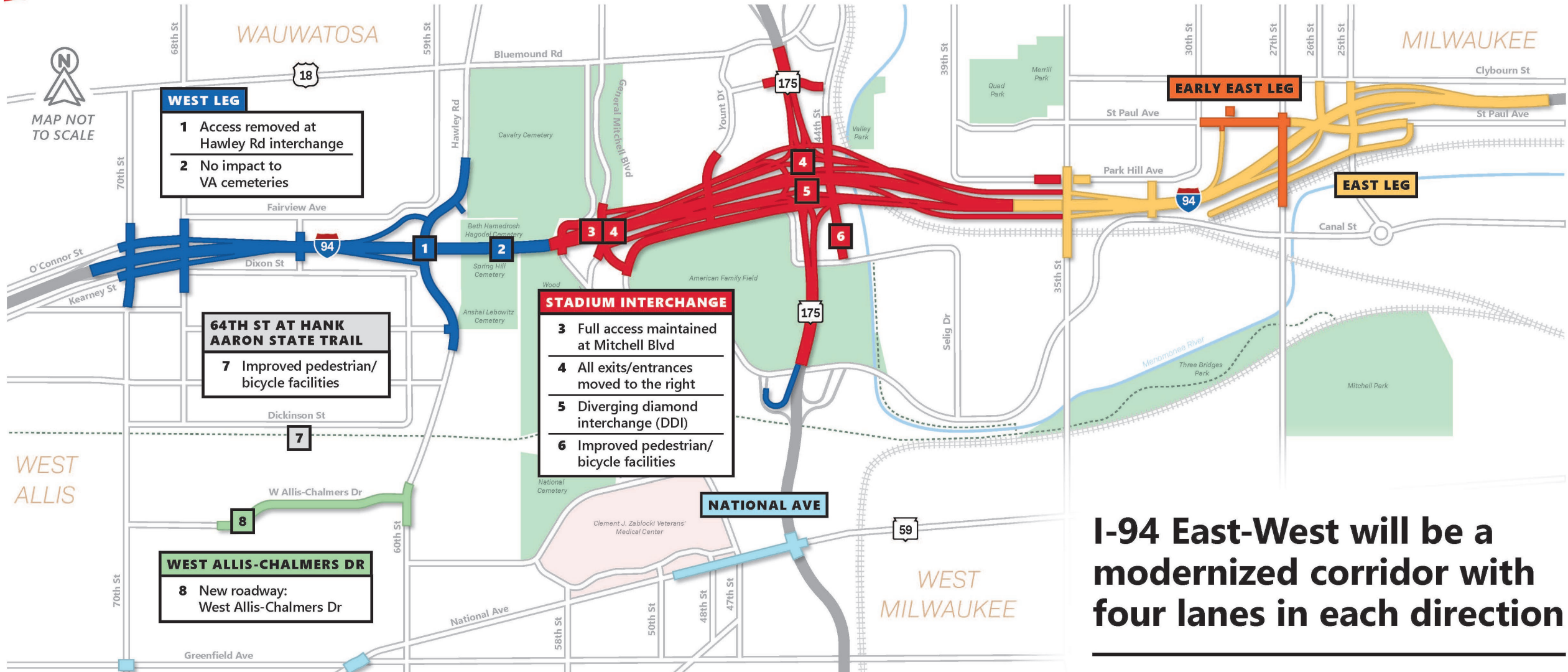
# Public outreach and engagement

- Hundreds of meetings and events
- Conversations with tens of thousands of stakeholders



# PLAN MOVING FORWARD

## PLAN MOVING FORWARD



**I-94 East-West will be a modernized corridor with four lanes in each direction**

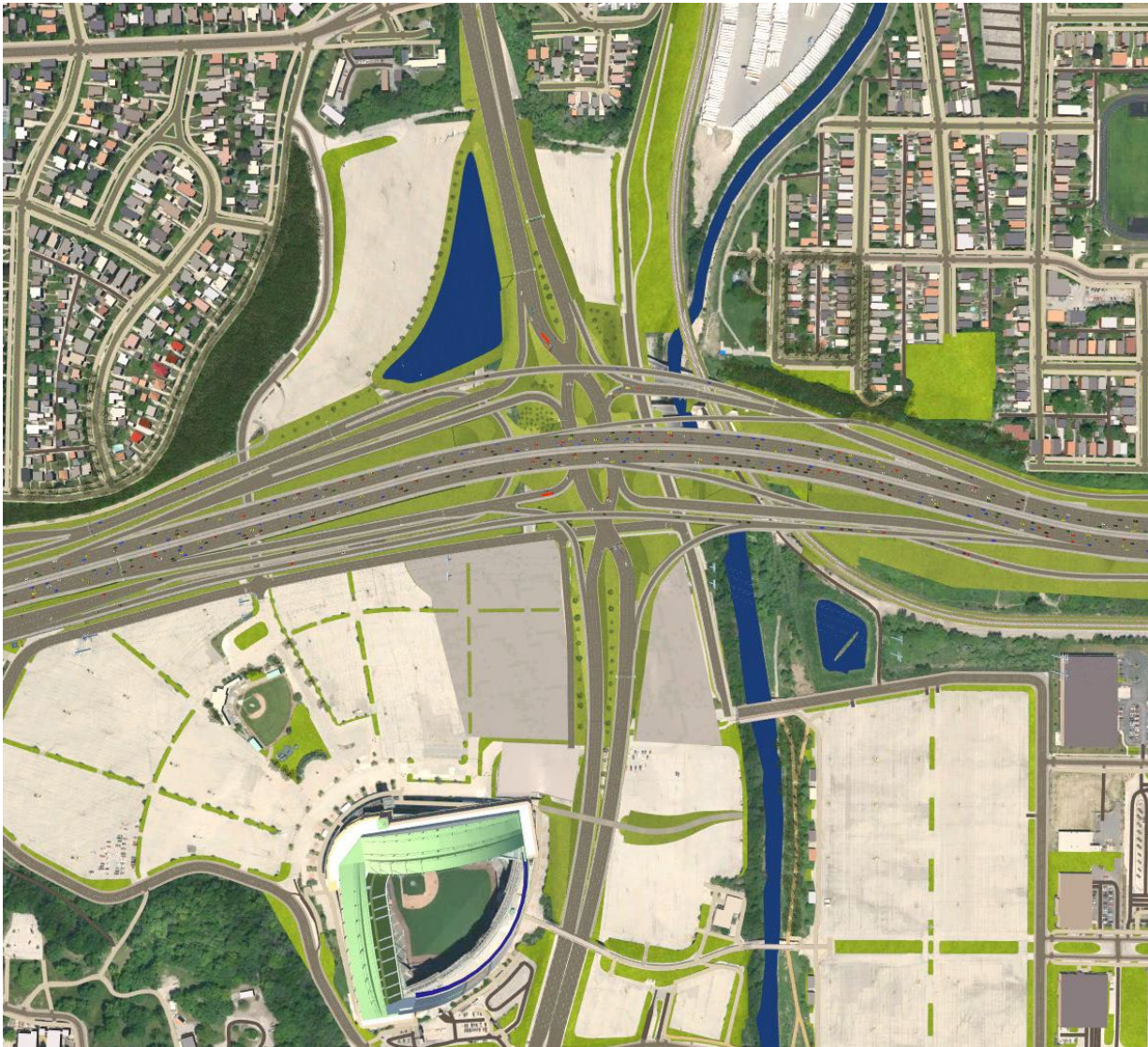


# STADIUM INTERCHANGE

- Stadium Interchange: right-sized to a Diverging Diamond Interchange
- Modern design standards placing all entrance/exit movements on right hand side
- Moved south away from neighborhoods



# STADIUM INTERCHANGE



- WIS 175
  - No longer system interchange
  - Designed as a Diverging Diamond Interchange
  - 2-signals on WIS 175 and signals for I-94 exits at WIS 175
- Includes “hook ramp” with direct access to General Mitchell Blvd. to accommodate local, VA, and Brewer traffic
- 2-levels above local roads

# CEMETERY SECTION OF CORRIDOR



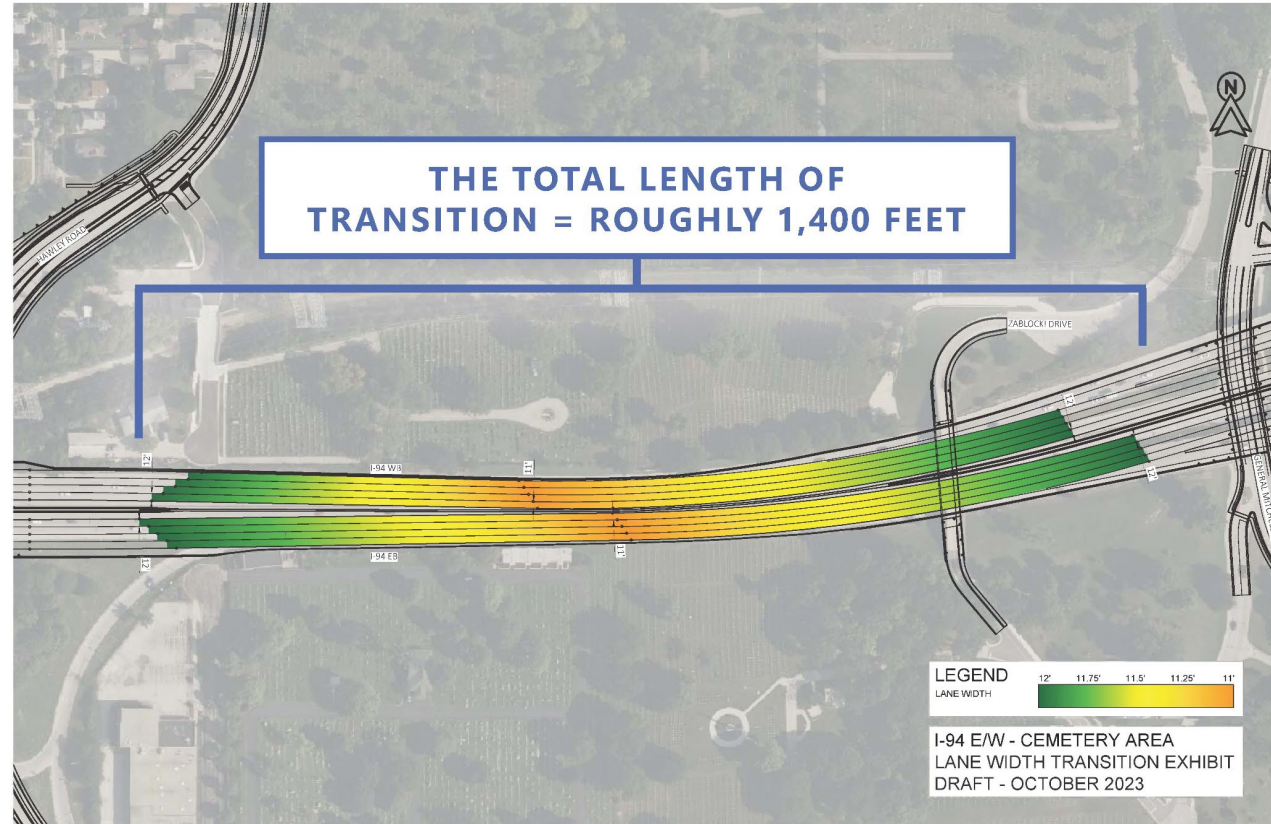
Lane width transitions from 12 feet, to 11 feet, and back to 12 feet.



At the narrowest point, where the lanes are 11 feet wide, driving through will be instantaneous.



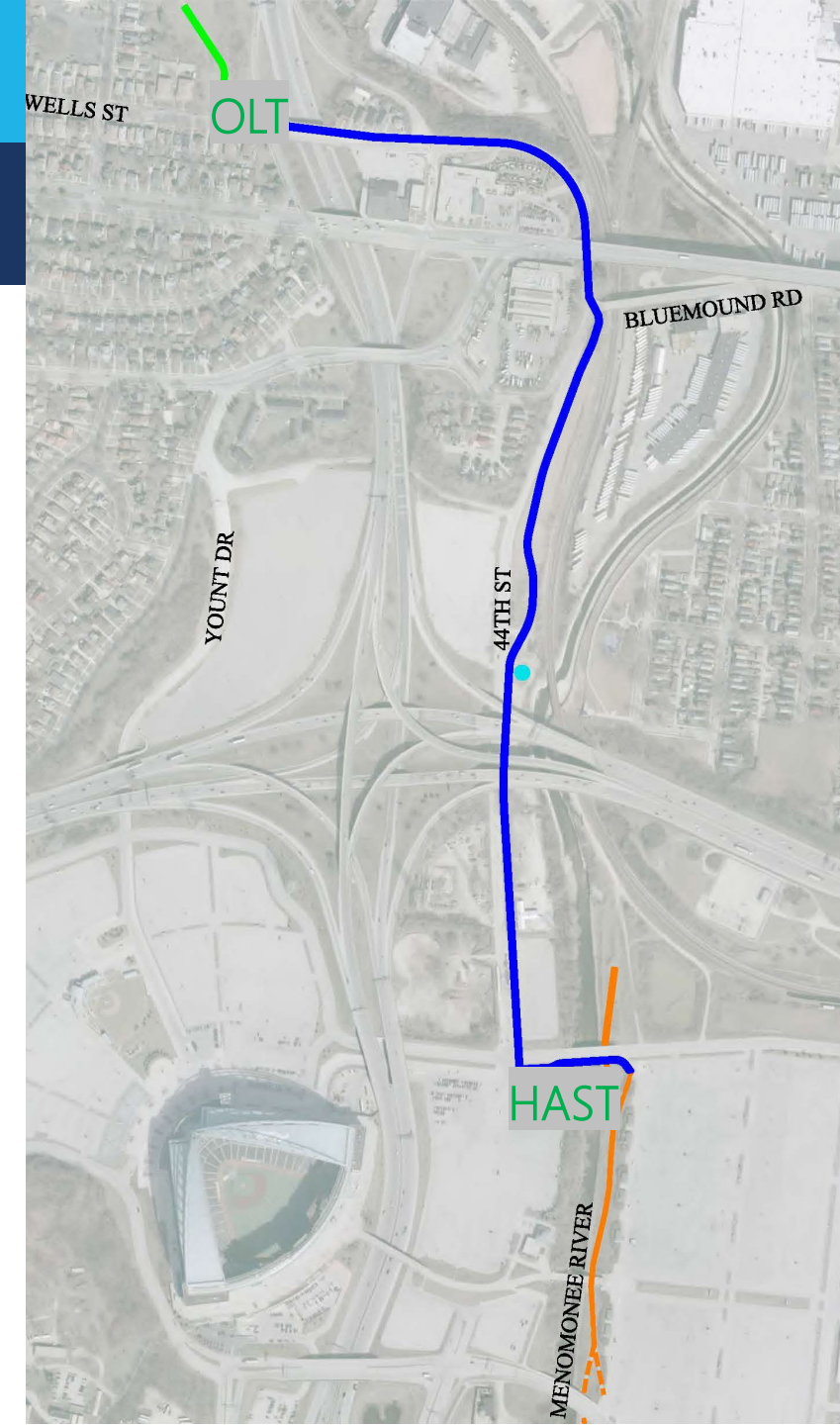
This section is a 17-second drive, traveling at 55 mph.



*11-foot lanes are used in other areas of Milwaukee and the State.*

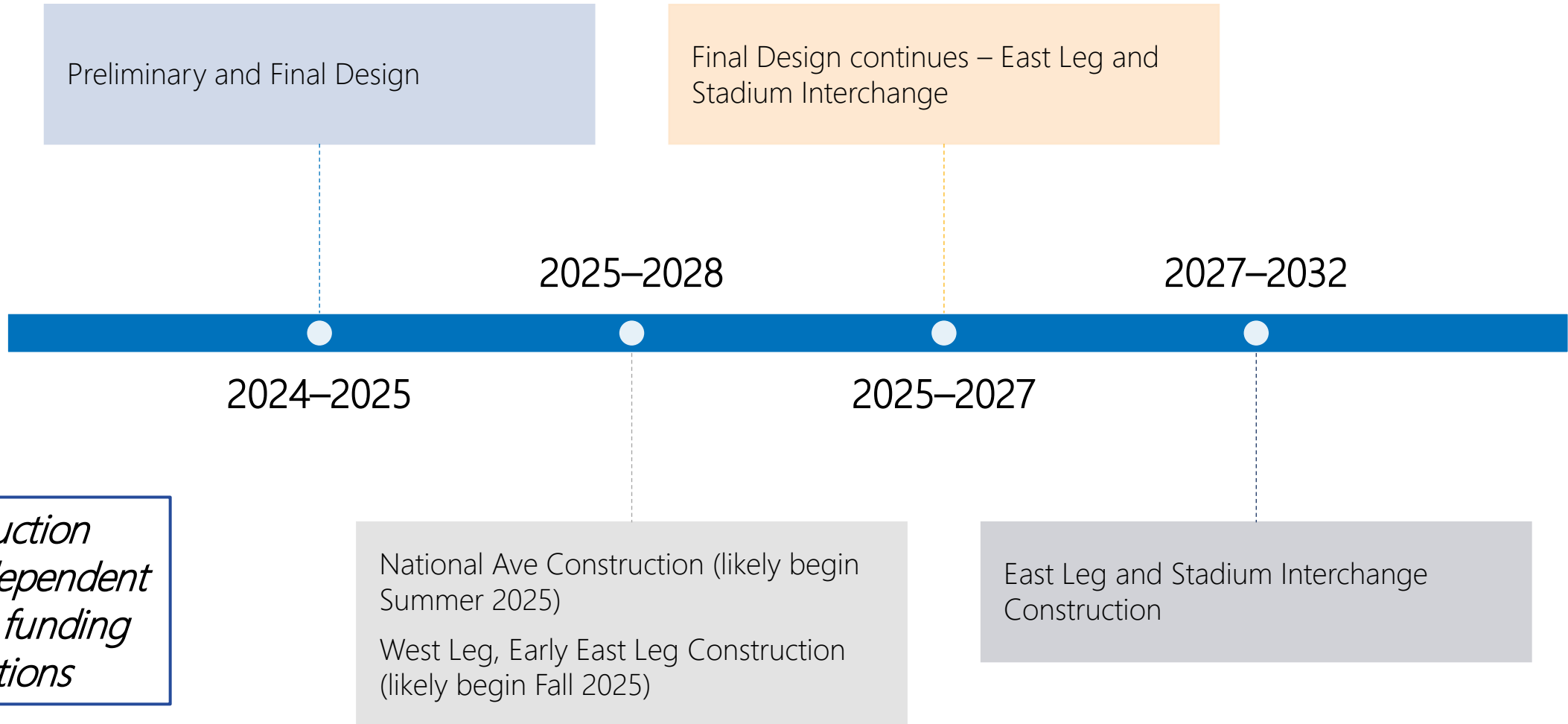
## MULTI-MODAL OPPORTUNITIES

- Stadium Interchange and East Leg
  - Connect Hank Aaron State Trail (HAST) and Oak Leaf Trail (OLT)
  - Connect north side neighborhoods to jobs in Valley
  - Safer, more inviting access points
  - Design elements to encourage vibrant neighborhood, discourage undesirable activity
- West Leg
  - Providing an additional HAST access at 64<sup>th</sup> Street
  - Hawley Road – removing conflicts on east side with half-interchange alternative
- Transit
  - Construction mitigation - \$25-\$30 Million for transit (NOTE: More than double the transit funding of previous WisDOT mega-projects)



# SCHEDULE AND STAGING

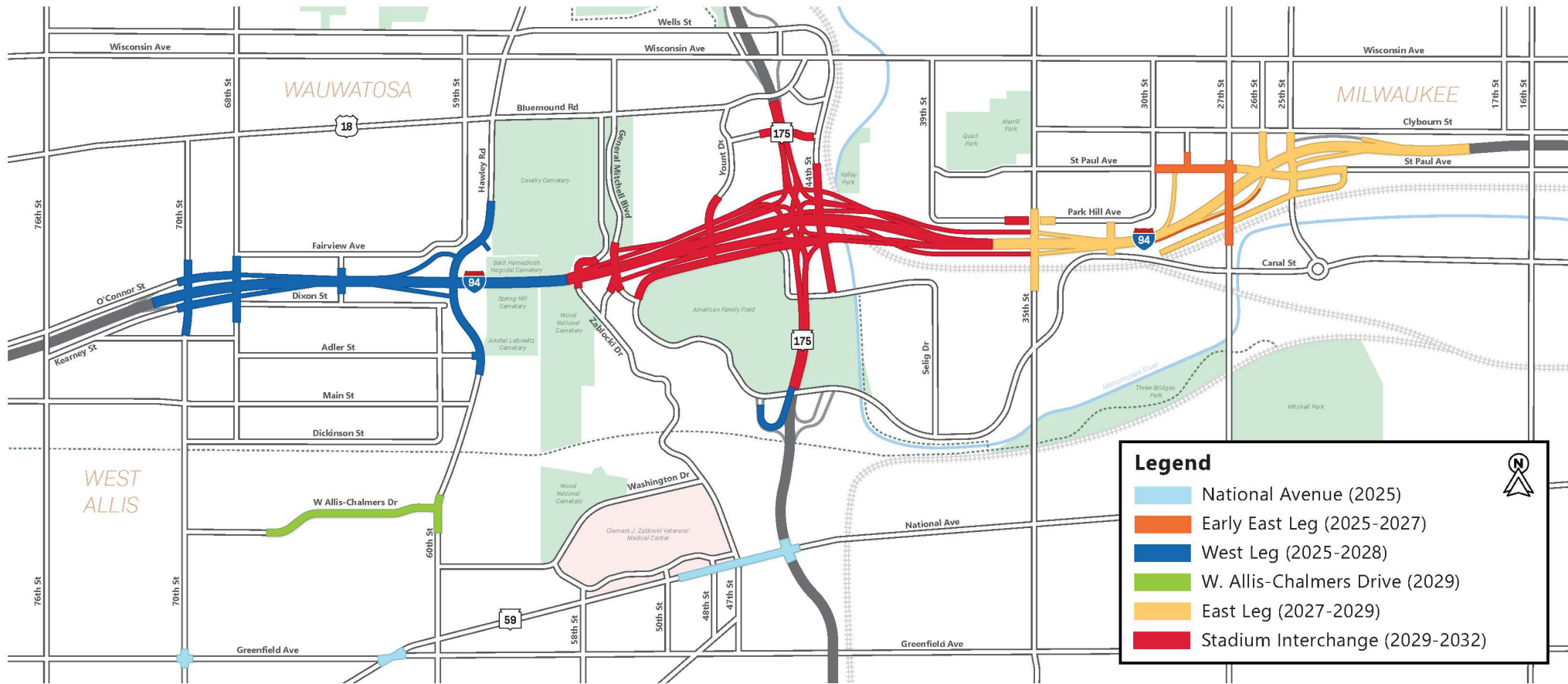
# ANTICIPATED SCHEDULE



*Construction schedule dependent on future funding allocations*

# CONSTRUCTION STAGING

The map below is the current plan for constructing and sequencing the reconstruction of the full I-94 East-West corridor. Many variables impact this plan including accommodating existing traffic, utility relocation, and available funding. This plan will continue to be refined and is subject to change as the design progresses.



# PROGRAM OVERVIEW and SCHEDULE

- DRAFT - high potential for change
- Flexibility moving forward – funding, staging, access

Let Package	ID	Letting Date	Major Construction Complete
National Ave Mitigation	2410-47-70	5/13/2025	Late 2025
Early East Leg	1060-27-74	9/9/2025	Mid 2027
West Leg	1060-27-71	9/9/2025	Late 2027
East Leg*	1060-27-72	9/14/2027	Late 2029
Allis Chalmers Drive	2995-27-70	3/14/2028	Mid 2029
Stadium Interchange - Mainline*	1060-27-73	9/11/2029	2032
Stadium Interchange - WIS 175*	1060-27-73	9/9/2031	2032

*\*Package Details and Dates Subject to Change – November 2024*



## WEST LEG



ENLARGED MAP ABOVE



### PROJECT LIMITS:

I-94 from 70th St to Zablocki Dr

### TIMELINE:

late 2025–late 2027

### PERMANENT TRAFFIC IMPACTS:

Permanent ramp closures at Hawley Rd

- > I-94 westbound exit ramp to Hawley Rd
- > Hawley Rd entrance ramp to I-94 eastbound

### CONSTRUCTION TRAFFIC IMPACTS:

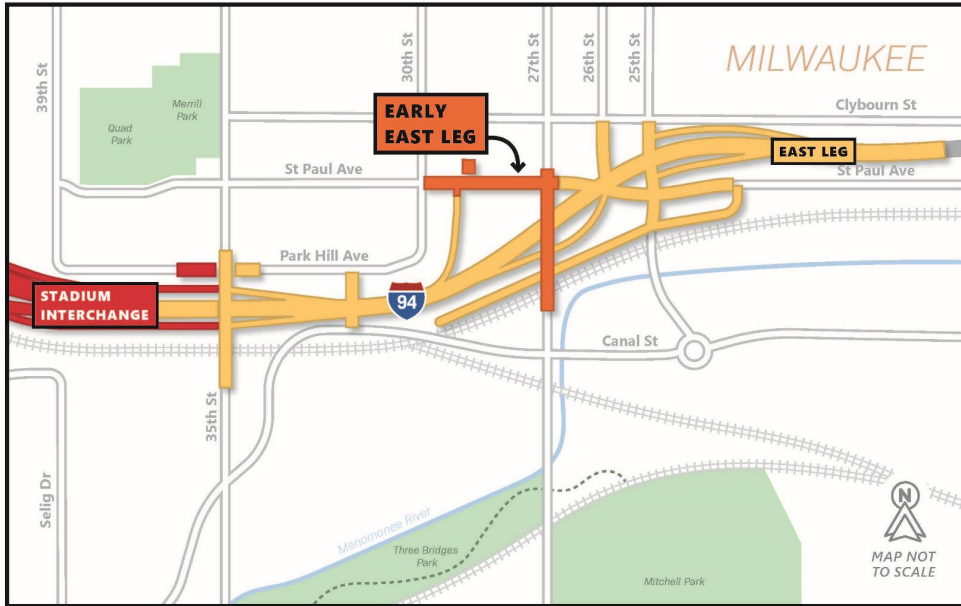
- > Long-term I-94 lane closures
- > Other I-94 entrance/exit ramp closures
- > Local road closures of 70th St, 68th St, 64th St, and Hawley Rd

# WEST LEG LET PACKAGE

I-94 EW: West Leg	
Quantities	
Item	Total
Excavation Common (CY)	150,000
Roadway Embankment (CY)	50,000
Temporary Shoring (SF)	50,000
Base Aggregate (Ton)	75,000
Select Crushed (Ton)	145,000
Concrete Pavement (SY)	130,000
HMA Pavement (Ton)	15,000
Concrete Barrier Temporary (LF)	80,000
Concrete Barrier (LF)	22,000
Bridge Deck (SF)	62,000
Retaining Wall (SF)	93,000

*\*Source: November 2024  
Contractor's Workshop*

## EARLY EAST LEG



### PROJECT LIMITS:

I-94 at 27th St

### TIMELINE:

late 2025–late 2026

### CONSTRUCTION TRAFFIC IMPACTS:

Long-term I-94 lane closures

Long-term closures of 27th St over I-94

I-94 entrance/exit ramp closures

> 35th St entrance ramp to I-94 eastbound (staging)

> I-94 eastbound exit ramp to 26th St

> 28th St entrance ramp to I-94 westbound

Closure of St Paul Ave between 27th St and 28th St

# EARLY EAST LEG LET PACKAGE

## I-94 EW: Early East Leg

### Quantities

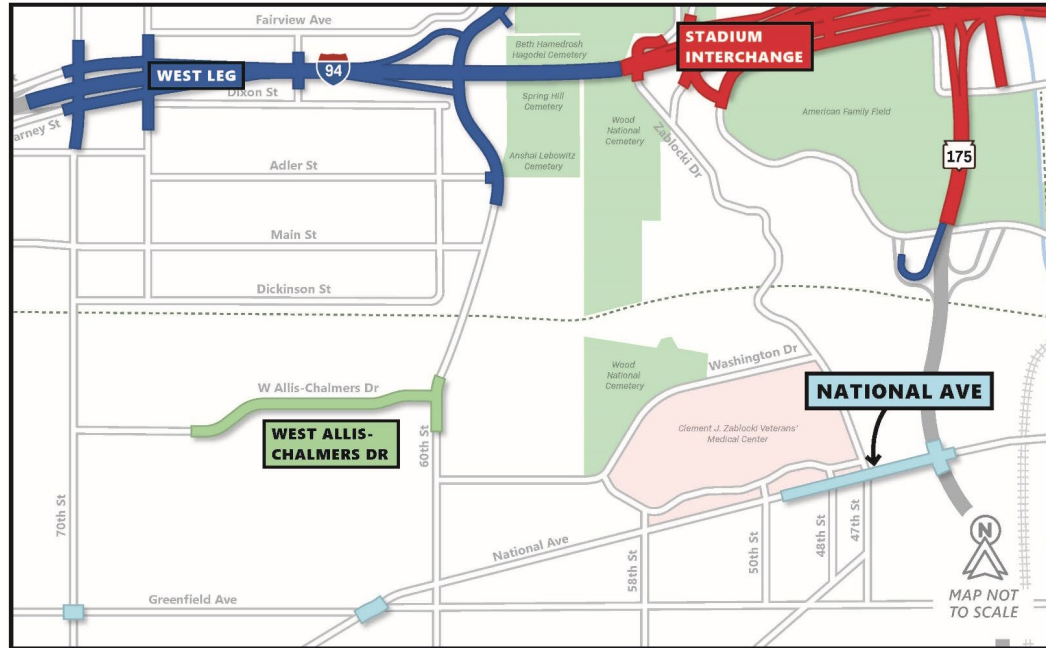
Item	Total
Excavation Common (CY)	100,000
Roadway Embankment (CY)	120,000
Temporary Shoring (SF)	12,000
Base Aggregate (Ton)	16,000
Select Crushed (Ton)	20,000
Concrete Pavement (SY)	7,500
HMA Pavement (Ton)	7,000
Concrete Barrier Temporary (LF)	20,000
Concrete Barrier Temporary Left in Place (LF)	5,000
Bridge Deck (SF)	35,000
Retaining Wall (SF)	69,000

*\*Source: November 2024  
Contractor's Workshop*

## EARLY EAST LEG LET PACKAGE– Structures

- Tiered Retaining Walls Between South Side of I-94 and Greves Street
  - Interim Completion Date(s)
  - Coordination with We Energies and ATC Required During Construction
  
- R-40-761 – Lower Tiered Wall
  - Primarily an Anchored Soldier Pile Retaining Wall
  - 1,745 Feet Long; ~30 Feet High
  - Long (Deep) Soldier Piles; +80 Feet Long; Socketed Into Rock
  - 3 Rows of Tieback Anchors; High Capacity (~280 kips); +100 feet Anchor Lengths
  - Includes an MSE Retaining Wall Unit at East End; Ground Improvement Likely Required
  
- R-40-767 – Upper Tiered Wall
  - MSE Retaining Wall
  - 1,300 Feet Long; ~25 Feet High
  - Lightweight Foamed Concrete Backfill Required In Some Areas

# NATIONAL AVE - MITIGATION



ENLARGED MAP ABOVE



## PROJECT PURPOSE:

Improve local routes due to permanent Hawley Rd ramp closures

## PROJECT LIMITS:

Various locations

- > National Ave from Miller Park Way to 50th St
- > National Ave and Miller Park Way
- > National Ave and Greenfield Ave
- > Greenfield Ave and 70th St

## TRAFFIC IMPACTS:

- > Long-term lane closures at National Ave and Miller Park Way
- > Off-peak lane closures for other locations

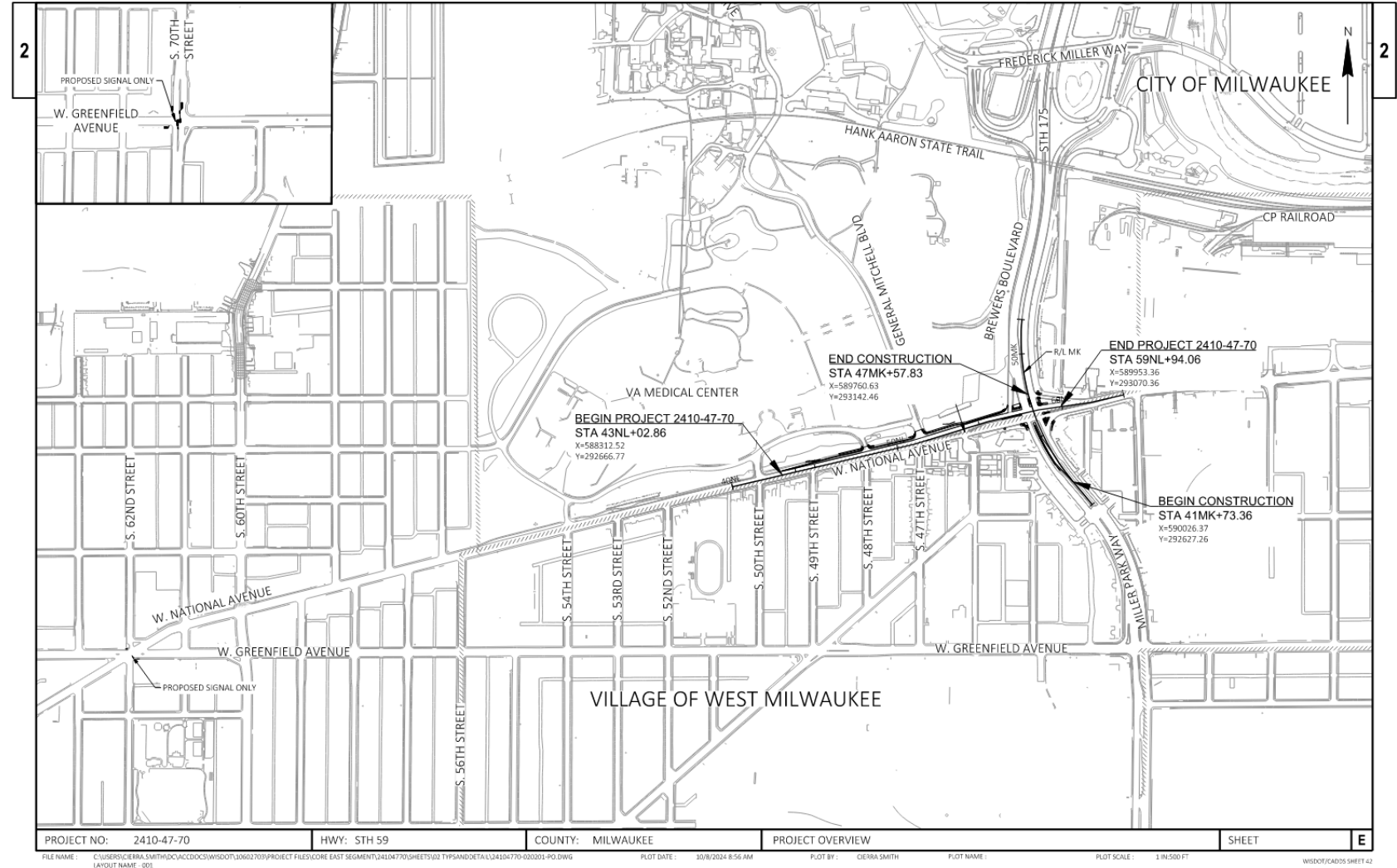
## TIMELINE:

Summer/Fall 2025

## NATIONAL AVENUE MITIGATION IMPROVEMENTS LET PACKAGE



- May 2025 Letting
- Addition of NB to WB Left Turn Lane at National Ave & Miller Park Way
- Curb ramp improvements
- Signal Improvements
  - National Ave & Miller Park Way
  - National Ave & 47<sup>th</sup> St, 48<sup>th</sup> St
  - Greenfield Ave & National Ave, 70<sup>th</sup> St.



# LOCAL BUSINESSES BENEFIT FROM WORK

- Bridge and ironwork
- Construction support and inspection
- Trucking
- Erosion control
- Traffic control
- Landscaping
- Supplies
- Signage
- Painting
- Staining
- Concrete barrier wall
- Lighting and traffic signals

*DBEs are almost always  
Wisconsin-based businesses*



## Disadvantaged business opportunities are plentiful





**Don't forget to fill out  
the evaluation for this session.**

These evaluations help shape future events.

**THANK YOU**